

AMENDMENT NO.____

TO THE OFFICIAL PLAN OF THE
TOWN OF BRADFORD WEST
Gwillimbury

TOWN OF BRADFORD WEST Gwillimbury TOWN-INITIATED OFFICIAL PLAN
AMENDMENT

TOWN-WIDE

ADOPTED:_____, 2025

APPROVED BY THE COUNTY OF SIMCOE:_____,2025

IN EFFECT:_____, 2025

AMENDMENT NO.

TO THE OFFICIAL PLAN OF THE TOWN OF
BRADFORD WEST GWILLIMBURY

The attached explanatory text constituting Amendment No. ___ to the Official Plan for the Town of Bradford West Gwillimbury, 2021 was adopted by the Council of the Corporation of the Town of Bradford West Gwillimbury by By-law 2025-__ in accordance with the provisions of Sections 17 and 22 of the Planning Act, R.S.O. 1990, c. P.13 on the ___ day of _____, 20__.

James Leduc, Mayor

Tara Reynolds, Clerk

CORPORATE SEAL OF MUNICIPALITY

Corporation of the Town of Bradford West

Gwillimbury BY-LAW 2025-##

Being a By-law to adopt Amendment No. # to the Official Plan for the Town of Bradford West Gwillimbury.

The Council of the Corporation of the Town of Bradford West Gwillimbury in accordance with the provisions of the Planning Act, R.S.O. 1990, c. P.13, hereby enacts as follows:

1. The attached explanatory text which constitutes Amendment No. # to the Official Plan for the Town of Bradford West Gwillimbury is hereby adopted.
2. The Clerk is hereby authorized and directed to make application to the County of Simcoe for approval of Amendment No. # to the Official Plan for the Town of Bradford West Gwillimbury.
3. This By-law shall come into force and take effect on the day of final passing thereof.

Enacted this_day_____, 20__.

James Leduc, Mayor

Tara Reynolds, Clerk

AMENDMENT NO. _____
TO THE OFFICIAL PLAN OF THE
TOWN OF BRADFORD WEST
GWILLIMBURY

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THE CONSTITUTIONAL STATEMENT

The following Amendment to the Official Plan for the Town of Bradford West Gwillimbury consists of three parts:

PART A – THE PREAMBLE – consists of the purpose, location and basis for the Amendment and does not constitute part of the actual Amendment;

PART B – THE AMENDMENT – which sets out the actual Amendment constitutes Amendment No. to the Official Plan for the Town of Bradford West Gwillimbury; and,

PART C – THE APPENDIX – consists of information pertinent to this Amendment in the form of a record of the Council of the Corporation of the Town of Bradford West Gwillimbury actions, and does not constitute a part of the actual Amendment.

PART A – THE PREAMBLE

1.0 PURPOSE

The purpose of this Amendment is to delineate the boundary and establish the associated policy framework for the Bradford GO *Major Transit Station Area* (MTSA). The Amendment implements and gives effect to the policy direction of the County of Simcoe Official Plan Amendment No. 7 (SCOPA 7), as adopted by County Council, which identifies the Bradford GO MTSA as a strategic location for intensification and transit-supportive development. It also ensures that the Town’s Official Plan remains in conformity with the requirements of Section 26 of the *Planning Act, R.S.O. 1990, c. P.13*, as amended, and in consistency with the *Provincial Planning Statement, 2024* (PPS 2024).

Through this Amendment, the Town will establish the MTSA boundary and policies in order to guide the form and scale of development within the MTSA in order to support efficient land use, optimize existing and planned transit infrastructure, and accommodate population and employment growth in a manner consistent with provincial and County planning objectives.

2.0 LOCATION

The lands subject to this Amendment (the “Subject Lands”) are located within the designated Settlement Area of the Town of Bradford West Gwillimbury, as identified in Schedule A – Land Use Plan of the Town’s Official Plan. The Subject Lands comprise the area surrounding the Bradford GO Station, encompassing a total area of approximately 62 hectares within an 800-metre radius of the station, which is situated near the intersection of Dissette Street, Holland Street West, and Bridge Street.

The delineation of the MTSA boundary corresponds to the PPS, 2024, the Bradford GO Station Area identified in the County Council adopted SCOPA 7 and has been refined through detailed local analysis. The boundary, as established through this Amendment, is illustrated on Schedule ‘1’ – Bradford GO Major Transit Station Area Boundary, which forms part of this Amendment.

3.0 BASIS

The basis for this Amendment arises from the Town of Bradford West Gwillimbury’s ongoing Growth Management Strategy (GMS) and Official Plan Review, undertaken pursuant to Section 26 of the *Planning Act* to ensure continued conformity and consistency with upper-tier and provincial planning policy. The GMS establishes a comprehensive framework for accommodating population and employment growth to the 2051 planning horizon, as allocated through the County Council adopted, County of Simcoe Official Plan Amendment No. 7 (SCOPA 7) and consistent with the Provincial Planning Statement, 2024 (PPS 2024).

As part of this policy framework, the Bradford GO Station Area was identified as a Major Transit Station Area (MTSA) and a strategic growth node to support compact, mixed-use, and transit-oriented development. The delineation of the MTSA boundary and the

establishment of implementing policies are necessary to:

- direct higher-density residential and employment uses to a location served by existing and planned GO Transit rail infrastructure;
- promote complete communities with a range and mix of housing, jobs, services, and public spaces within a walkable urban structure;
- optimize land use and infrastructure efficiency consistent with PPS 2024 Policy 2.3.1 and Policy 2.4.2;
- support municipal fiscal sustainability and climate-resilient development patterns;
- Amend Schedule 'B-1' to the Town OP to identify the Bradford GO Major Transit Station Area, the Holland Street West Strategic Growth Area overlay; and,
- Amend Schedule C to the Town OP to establish new connections and remove notations.

More specifically, the proposed amendment is based on the following considerations:

3.1 The Provincial Planning Statement, 2024

The *Provincial Planning Statement, 2024* (PPS 2024) provides policy direction on matters of provincial interest related to land use planning and development under the authority of the *Planning Act*. The PPS 2024 establishes the policy foundation for regulating the development and use of land in a manner that supports the provincial goal of improving the quality of life for Ontarians through the promotion of efficient, sustainable, and resilient land use patterns.

The PPS 2024 came into effect on October 20, 2024, replacing both the *Provincial Policy Statement, 2020* and *A Place to Grow: Growth Plan for the Greater Golden Horseshoe* (2019, as amended). Pursuant to Section 3(5) of the *Planning Act*, all decisions affecting planning matters "shall be consistent with" policy statements issued under the Act, including the PPS 2024. The PPS 2024 requires municipalities to plan for a minimum 20-year time horizon when adopting a new Official Plan or conducting a review under Section 26 of the *Planning Act* and to maintain a 15-year supply of land designated and available for residential development.

In accordance with Policy 2.3.1 of the PPS 2024, growth and development are to be directed to settlement areas, with a focus on compact, mixed-use, and transit-supportive forms of development. Within settlement areas, the PPS identifies Major Transit Station Areas (MTSAs) as strategic locations for accommodating higher densities and supporting multimodal transportation access.

Policy 2.4.2 of the PPS 2024 provides that MTSAs served by commuter or regional rail shall be delineated within a radius of 500 to 800 metres from the station and planned to achieve a minimum density target of 150 residents and jobs combined per hectare. Development and intensification within MTSAs are to be supported through the promotion of land uses and built forms that:

- achieve the prescribed density targets;
- support complete communities;
- enable efficient transit use; and

- facilitate multimodal access and integration.

PPS 2024 also provides direction regarding natural hazards. Pursuant to Policy 4.2, development is generally to be directed outside of hazardous lands such as floodplains and erosion-prone areas. Institutional uses, essential emergency services, and uses associated with hazardous substances are prohibited within such areas.

The proposed Amendment facilitates residential and mixed-use intensification within the Town's Settlement Area through the establishment of the Bradford GO Major Transit Station Area. It demonstrates consistency with the PPS 2024 by:

- directing growth to a provincially identified strategic growth area, known as the Major Transit Station Area;
- achieving transit-supportive density and compact form;
- supporting complete community objectives; and
- respecting the natural hazard policies of the PPS by recognizing floodplain constraints along Bridge Street and ensuring that development is directed outside of hazardous areas.

3.2 The Greenbelt Plan, 2017

The Greenbelt Plan, 2017 established under the authority of the *Greenbelt Act*, 2005, provides a comprehensive policy framework for the long-term protection of Ontario's agricultural land base, natural heritage systems, and water resource systems within the Greenbelt Area. Pursuant to Section 3(5)(b) of the *Planning Act*, municipal planning decisions are required to conform with applicable provincial plans, including the Greenbelt Plan.

Within the southwestern portion of the Town of Bradford West Gwillimbury, a significant area of the municipality lies within the Greenbelt Area, designated as Protected Countryside under the Plan. This area includes lands within the Holland Marsh, identified as a Specialty Crop Area of provincial significance, recognized for its intensive vegetable production and contributions to Ontario's agri-food economy. The Greenbelt Plan requires that the entire land base of the Specialty Crop Area be protected for long-term agricultural use while permitting agriculturally supportive infrastructure and value-added activities that sustain the economic viability and character of these regions.

In conformity with the Greenbelt Plan, none of the lands within the Greenbelt Area or the Protected Countryside designation have been included within the Bradford GO Major Transit Station Area (MTSA). The boundary of the MTSA, as delineated through this Amendment, is entirely situated within the Town's designated Settlement Area, outside of the Greenbelt Area.

Accordingly, this Amendment is in full conformity with the Greenbelt Plan, consistent with Section 3 of the Planning Act and the broader intent of protecting Ontario's agricultural and natural systems.

3.3 The Lake Simcoe Protection Plan, 2009

The Lake Simcoe Protection Plan (LSPP) was prepared and approved under the authority of the *Lake Simcoe Protection Act, 2008* (LSPA) and came into effect on June 2, 2009. The LSPP applies to the Lake Simcoe watershed, which encompasses Lake Simcoe and all surrounding areas from which surface water drains into the lake. The General Regulation (O. Reg. 60/09) made under the LSPA delineates the watershed boundary, a significant portion of which lies within the Town of Bradford West Gwillimbury. The LSPP establishes a comprehensive framework to protect, improve, and restore the ecological health of the Lake Simcoe watershed. It sets out objectives, targets, and policies addressing a wide range of environmental components, including:

- aquatic life and habitat protection,
- water quality and quantity,
- shoreline management,
- natural heritage systems,
- invasive species prevention,
- climate change mitigation and adaptation, and
- land and water-based recreation and implementation measures.

Pursuant to Section 1(1) of the *Planning Act*, the LSPP is recognized as a Provincial Plan, and all land use planning decisions within the watershed must conform to its applicable policies. The PPS 2024 further complements the LSPP by providing direction on matters of provincial interest related to natural heritage, water resources, and natural hazards, thereby reinforcing the objectives of watershed protection and sustainable land use. The proposed Amendment is consistent with the intent and policies of the LSPP by:

- directing growth and intensification to the designated MTSA,
- incorporating the appropriate permissions for the floodplain on Bridge Street, and
- ensuring that future development within the MTSA integrates watershed protection measures in accordance with LSPP objectives.

Accordingly, this Amendment conforms with the Lake Simcoe Protection Plan and supports its long-term goal of improving and sustaining the ecological integrity of the Lake Simcoe watershed by incorporating and or referencing the applicable policies and requirements of the LSPP within the policies.

3.4 South Georgian Bay–Lake Simcoe Source Protection Region Plan, 2014

The South Georgian Bay–Lake Simcoe Source Protection Region Plan (2014) (SBLSSP) was prepared and approved under the authority of the *Clean Water Act, 2006*. The Plan provides a comprehensive, preventative, and watershed-based framework for protecting existing and future sources of municipal drinking water across the region. The *Clean Water Act* introduced a proactive approach to safeguarding Ontario's drinking water resources at the source, prior to treatment and distribution.

Ontario Regulation 284/07, made under the *Clean Water Act*, establishes the boundaries of Source Protection Areas to align with watershed boundaries. The South Georgian Bay–Lake Simcoe Source Protection Region includes four major watersheds: Lake Simcoe, Nottawasaga Valley, Black-Severn, and Severn Sound. The Town of Bradford West Gwillimbury is situated within both the Lake Simcoe and Nottawasaga Valley watersheds. As such, continued collaboration with the Lake Simcoe Region Conservation Authority and the Nottawasaga Valley Conservation Authority is essential to ensure the integration of source protection policies into municipal land use planning processes.

Pursuant to the *Clean Water Act*, municipalities are required to ensure that Official Plan policies conform with the significant drinking water threat policies contained within the approved Source Protection Plan. These policies apply to designated vulnerable areas, including Wellhead Protection Areas (WHPAs) and Intake Protection Zones (IPZs), where specific land uses may pose risks to the quality or quantity of municipal drinking water. The *Planning Act* further supports this integration by identifying the protection of public health and safety and the conservation of natural resources as matters of provincial interest under Section 2. In addition, the *Provincial Planning Statement, 2024 (PPS 2024)* reinforces the requirement for municipalities to incorporate source water protection objectives into local land use planning frameworks.

In the context of the Town’s Growth Management Strategy and Official Plan Review to 2051, the Source Protection Plan represents a key environmental and policy constraint. As the Town accommodates future population and employment growth, development must be directed and managed to ensure the ongoing protection of vulnerable drinking water sources. Locally mapped WHPAs, including those associated with the Town’s municipal well systems, must be carefully considered in identifying areas for intensification and expansion. Potential risks associated with future development must be evaluated and appropriate risk management measures applied where necessary.

Integrating source protection policies into the Town’s Official Plan is essential to ensuring long-term public health, environmental sustainability, and conformity with both the *Clean Water Act, 2006* and the *Planning Act*, while supporting the regional objectives of watershed-based planning and sustainable growth. The requirements of the SGBLSSP are reflected in the Official Plan policies applicable to the MTSA. As such this amendment conforms to the SGBLSSP.

3.5 Simcoe County Official Plan, 2016

The Simcoe County Official Plan, 2016 (“SCOP”) sets out policies to protect and enhance the County’s natural and cultural heritage, manage the County’s resources, implement efficient growth, coordinate land use planning among the local municipalities, ensure economic sustainability and promote public health and safety. The settlement areas of Bradford and Bond Head are designated “Settlements” as per Schedule 5.1 – Land Use Designations in the SCOP while Bradford is identified as a Primary Settlement Areas on Schedule 5.1.2 – Simcoe Sub Area. Section 3.5 directs that the Settlements designation is intended to be

the focus of population and employment growth in the Municipality. Further, the settlement area should be developed with a compact urban form that promotes the efficient use of land and services and promotes a mix of uses to create a strong and vibrant central place.

The County of Simcoe Official Plan Amendment No. 7 (SCOPA 7), adopted by County Council on April 23, 2024 establishes the growth allocations and policy framework to accommodate population and employment growth to the 2051 planning horizon across the County. SCOPA 7 was prepared in accordance with Section 26 of the *Planning Act* and implements the population and employment forecasts and policies that were established by the now revoked *A Place to Grow: Growth Plan for the Greater Golden Horseshoe (2019)* and the *Provincial Policy Statement, 2020*. The Amendment provides direction on the distribution of growth to the County's lower-tier municipalities, including the Town of Bradford West Gwillimbury, based on a planning horizon to 2051 and a County-wide allocation of 416,000 residents and 137,000 jobs.

Within this framework, SCOPA 7 identifies the Bradford GO Major Transit Station Area (MTSA) as a strategic growth area intended to accommodate a significant share of the Town's future intensification and mixed-use development. The County's growth strategy emphasizes focusing intensification within Settlement Areas, particularly within MTSA's and other areas served by existing and planned transit infrastructure.

In conformity with SCOPA 7, this Amendment:

- establishes the boundary and policy framework for the Bradford GO MTSA consistent with the delineation endorsed by the County;
- provides the basis for achieving the minimum density target of 150 residents and jobs combined per hectare for MTSA's served by commuter rail, in accordance with PPS 2024 Policy 2.4.2;
- directs intensification to areas fully serviced by municipal infrastructure thereby supporting the efficient use of land and infrastructure; and
- ensures that the Town's Official Plan continues to conform with the County of Simcoe Official Plan, including the growth allocations, intensification hierarchy, and settlement structure established through SCOPA 7.

Accordingly, the Bradford GO MTSA policies contained in this Amendment are in full conformity with the County of Simcoe Official Plan, as well as the Council adopted Amendment No. 7 to the County Official Plan.

As required by Section 27(1) of the *Planning Act*, this amendment implements the County's direction to promote compact, transit-supportive, and complete communities across Simcoe County.

3.6 The Town of Bradford West Gwillimbury Official Plan

The Town of Bradford West Gwillimbury Official Plan (2023) came into effect on

March 14, 2023. The Official Plan serves as the Town's primary guiding document for managing land use and development, ensuring that growth appropriately balances the social, economic, cultural, and environmental interests of the community. The Plan establishes goals and objectives intended to promote a complete, resilient, and sustainable community by directing growth in an orderly and efficient manner.

Policy 3.1(b) identifies the Bradford Urban Area as the Town's primary Settlement Area, where the majority of growth and development is to be directed. Subsection 3.2.1 further clarifies that intensification is permitted throughout the delineated Built-Up Area, with the Downtown Bradford Urban Centre serving as the primary focus for higher-density, mixed-use development. Lands outside of Downtown Bradford are encouraged to develop in a compact, transit-supportive form to advance the Town's complete community objectives.

An amendment to the Town's Official Plan is required to:

- establish a planning horizon to the year 2051, consistent with the County of Simcoe Official Plan Amendment No. 7 (SCOPA 7) and the Provincial Planning Statement, 2024 (PPS 2024);
- ensure conformity with recent amendments to the *Planning Act* and related provincial policy direction; and
- designate and define Strategic Growth Areas (SGAs), including the Bradford GO Major Transit Station Area (MTSA);

This Amendment will therefore establish the Bradford GO MTSA as one of the Town's Strategic Growth Areas, consistent with the Town's vision to direct intensification to key nodes and corridors that support transit investment, housing diversity, and economic vitality. Based on the foregoing, the proposed Amendment is consistent with the intent and objectives of the Town of Bradford West Gwillimbury Official Plan (2023) and serves to update the Plan in accordance with Section 26 of the Planning Act to ensure continued conformity with applicable provincial and County policy frameworks.

This Amendment therefore provides the mechanism to implement the policies and direction for growth as established through County and Provincial policy. The MTSA boundary and policies collectively ensure that growth is directed in a manner that:

- maintains conformity with the County Official Plan;
- conforms with provincial plans including the Greenbelt Plan, 2017, the Lake Simcoe Protection Plan, 2008, and South Georgian Bay Lake Simcoe Source Protection Plan, 2015
- demonstrates consistency with the Provincial Planning Statement, 2024; and
- has regard to matters of provincial interest in accordance with Section 2 of the *Planning Act*.

PART B – THE AMENDMENT

4.0 INTRODUCTORY STATEMENT

“Part B – The Amendment” consisting of the Text changes provided in Table “1” below and Schedule “A” constitute Amendment No.____to the Official Plan of the Town of Bradford West Gwillimbury, 2025.

5.0 DETAILS OF THE AMENDMENT

The amendment includes both the changes to the text of the Town OP as described in the table below and changes to Schedule B-1A as described below.

Text Changes

The amendment includes the changes to the text of the Town of Bradford West Gwillimbury Official Plan, 2023 as described in the following table, and as provided in Attachment 2:

In the “Description of Change” column, text that is bolded is new text to be inserted into the Town OP. Text that is crossed out (“strikethrough”) is to be deleted from the Town OP.

The text of the Town OP is amended as follows:

Table “1”

Item No.	Section	Description of Change
1	Part B, Renumbered Policy 3.2.3.1 b)	The Bradford GO Station represents a significant asset with respect to attracting opportunities for intensification and is considered a Major Transit Station Area major transit station area . Anticipated future service and its location at the eastern edge of Downtown Bradford contribute to this facility as an important hub for all forms of transportation in the Town—as well as south Simcoe County and northwestern York Region.
2	Part B, New Policy 3.2.3.1 f)	Specific direction for planning in the major transit station area is provided through the policies of Special Policy Area 22 in this Plan.
3	Part E, New Section 6.22	***Insert New Section 6.22, Special Policy Area 22 – Bradford GO Major Transit Station Area with new policies as shown in Attachment 2 ***
N/A	Various	**The numbering of sections, subsections, and policies will be numbered accordingly**

Schedules Changes

Schedule B1-A within the Town OP is amended as follows:

- Schedule ‘B1-A’ is hereby amended by adding minimum and maximum height values for the four MTSA Corridor Areas (Bridge Street Corridor, Dissette Street Corridor,

Residential Edges, and Holland Street Corridor), the Holland Street Height Transition Overlay, the Bradford GO station location and MTSA boundaries, and the Holland Street West Strategic Growth Area boundaries

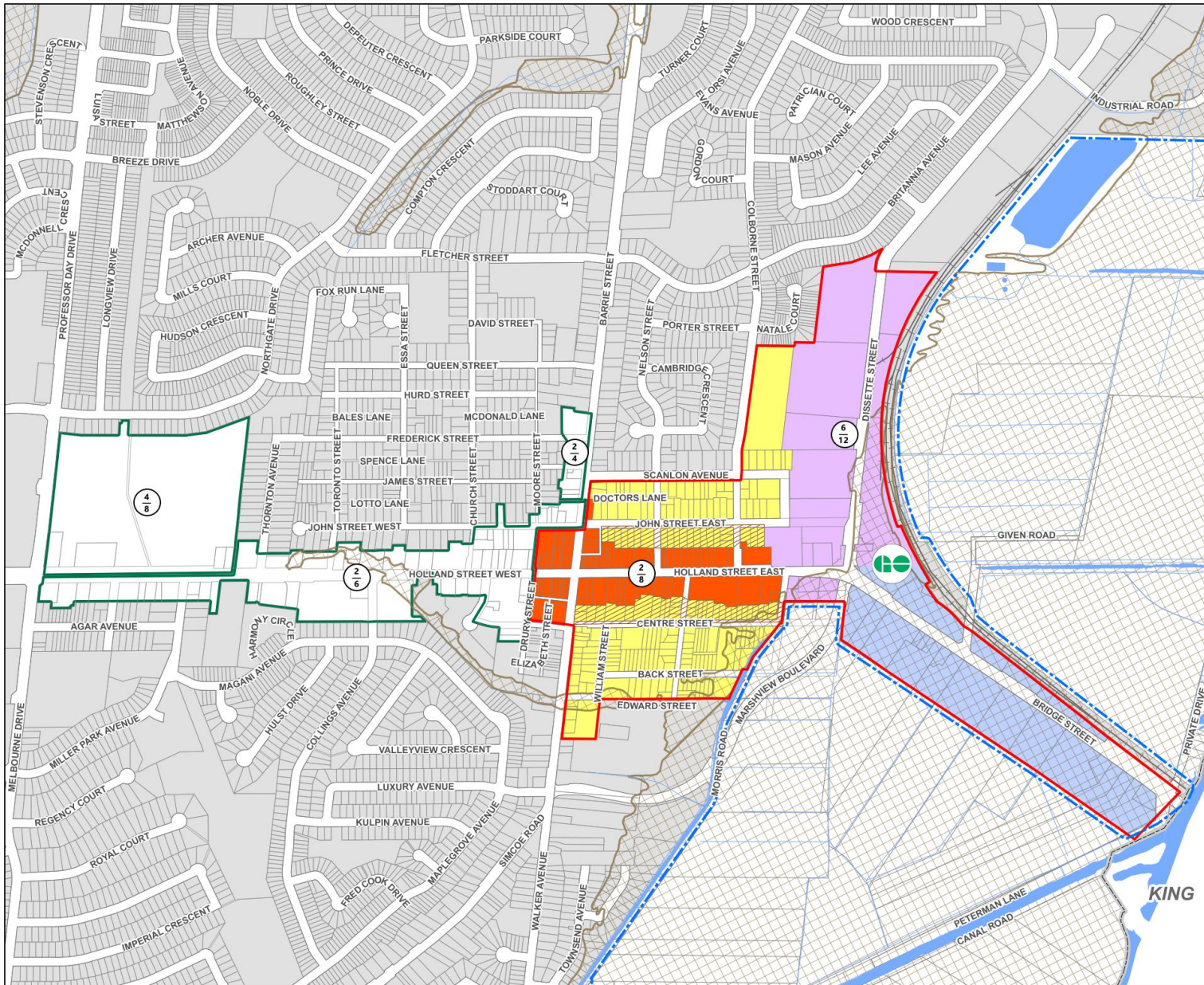
6.0 IMPLEMENTATION

The policies of this Amendment are to be implemented through the relevant policies in Section 7 – Implementing the Plan in the Town OP.

7.0 INTERPRETATION

The policies set forth in the Town OP, as amended from time to time, regarding the interpretation of that Plan, shall apply to this Amendment and as may more specifically be set out or implied within the policies contained therein. Unless precluded, altered, or exempted by any policies contained herein, all the relevant policies of the Town OP shall apply to this Amendment.

Town of Bradford West Gwillimbury
 Schedule "1" to Official Plan Amendment No. __



BRADFORD WEST GWILLIMBURY OFFICIAL PLAN *Building Our Tradition*
BRADFORD WEST GWILLIMBURY

**SCHEDULE B-1A
 DOWNTOWN BRADFORD
 STRATEGIC GROWTH AREAS**

- Settlement Area Boundary
- Major Transit Station Area (MTSA)
- Holland Street West SGA
- Bradford GO Station

MTSA Corridor Areas

- Bridge Street Corridor
- Dissette Street Corridor
- Residential Edges
- Holland Street Corridor
- Holland Street Height Transition Overlay
- Roads
- Railway
- Watercourses
- Floodplain (LSRCA)

Storey Height Legend:
 A B
 A represents the Minimum Number of Storeys
 B represents the Maximum Number of Storeys

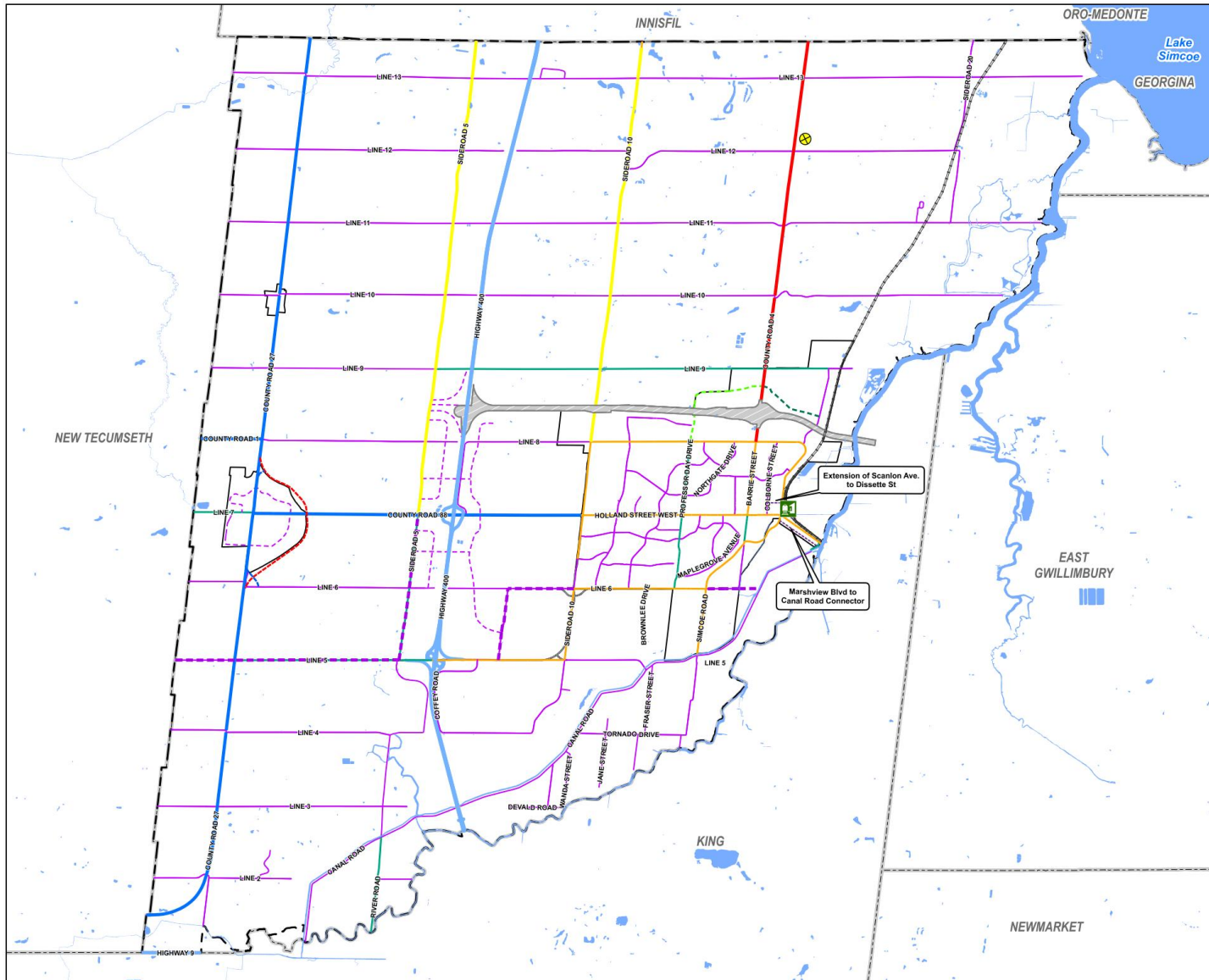
0 0.05 0.1 0.2 0.3 0.4 km

This is Schedule "1" to Official Plan Amendment No. __ as adopted by By-law 2025-__.

James Leduc, Mayor

Tara Reynolds, Clerk

Town of Bradford West Gwillimbury
 Schedule "2" to Official Plan Amendment No. ____

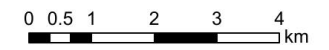


BRADFORD WEST GWILLIMBURY

**SCHEDULE C
 TRANSPORTATION PLAN**



- Bradford West Gwillimbury Boundary
- Higher Order Transit Station
- Railroad
- Town Road Classifications
 - Arterial-Major
 - Arterial-Minor
 - Collector
 - Potential - Arterial-Minor Road (OP 2002)
 - Potential - Arterial-Minor Road (OPA #17)
 - 2025 - Connection Concepts
- Town Road Classifications
 - Potential - Collector Road (OPA #15)
- County Road Classifications (County OP 2008)
 - Primary Arterial - Controlled Access
 - County Primary Arterial
 - Secondary Arterial
 - Potential Future County Road - 30.5m Road Width
 - Potential - County Road 27 Connector (OPA #16)
 - Potential - County Road 27 Bypass Road (OPA #16)
- Provincial Road Classifications (OP 2002)
 - Provincial Highway



Last Modified
 January 23, 2023 (per Modifications endorsed
 by Town Council on September 20, 2022)
 Adopted by Town Council
 March 2, 2021
 (By-law 2021-17)



This is Schedule "2" to Official Plan Amendment No. __ as adopted by By-law 2025-__.

James Leduc, Mayor

Tara Reynolds, Clerk

6.22 Special Policy Area 22 – Bradford GO Major Transit Station Area

Special Policy Area 22, as shown on Schedules B1-A of this Official Plan encompasses a total area of approximately 62 hectares located within an 800-metre (10-minute walking) radius of the Bradford GO Station, at the intersection of Dissette, Holland, and Bridge Streets. As a designated Major Transit Station Area (MTSA), this location is planned to accommodate the largest share of the Town’s growth to 2051, with a forecast of 12,800 people and jobs. The MTSA supports and implements the policy directions of the County of Simcoe Official Plan and the Provincial Planning Statement by targeting the achievement of a minimum density target of 150 residents and jobs per hectare.

The MTSA is envisioned as a compact, transit-supportive, and pedestrian-oriented community that will support and integrate a diverse mix of residential, commercial, institutional and recreational uses, supported by active transportation networks with accessible transit connections. This area will promote reduced automobile reliance and support the creation of complete and livable communities.

While the MTSA will accommodate the highest intensity of growth in the Town, development will not be uniform throughout. To recognize the distinct characteristics and development potential of its different parts, the area is structured into four distinct corridors—Bridge Street, Dissette Street, Holland Street, and the Residential Edges (north and south)—to reflect the unique characteristics and opportunities of each. Together, however, these distinct corridors contribute to a cohesive vision: a connected, vibrant, and complete community that balances small-town charm with urban vitality. Each corridor will maintain a distinct identity, guided by context-sensitive policies related to land use, built form, and density. This planning framework will guide development through to 2051, positioning the Bradford GO MTSA as both a destination and point of origin.

6.22.1 Vision

The Major Transit Station Area (MTSA) will evolve into a vibrant, transit-oriented community where Bradford’s small-town charm is thoughtfully and harmoniously integrated with compact, urban development. Anchored by the Bradford GO Station, the MTSA will become a dynamic hub offering a range of housing options, job opportunities, and access to

retail, cultural, and recreational amenities—all within a pedestrian-focused environment.

As the area grows, it will achieve higher densities and provide a range of housing options while preserving and celebrating Bradford's cultural heritage and historic character. Development will be shaped by strong urban design principles, including a network of pedestrian connections, multimodal transportation options, public spaces, and Privately Owned Public Spaces (POPs). Growth will be seamlessly integrated with surrounding agricultural and residential areas to ensure compatibility and cohesion.

The MTSA will serve as both a destination and a gateway—connecting residents to regional opportunities while creating a distinct sense of place that honours Bradford's past and embraces its future. Through thoughtful design and compact development, it will foster a human-scaled urban environment that supports a vibrant pedestrian realm—inviting people to walk, bike, work, gather, and play in spaces that feel connected, comfortable, and inclusive.

6.22.2 Goals and Objectives

As the Bradford Major Transit Station Area evolves, the Town will be guided by the following objectives:

- a) Create a distinctive community identity that accommodates growth while preserving and celebrating the Town's heritage.
- b) Establish a vibrant mixed-use community with an active streetscape, engaging public realm and a welcoming eastern gateway along the Bridge Street Corridor.
- c) Develop compact, complete communities with diverse and affordable housing options and job opportunities.
- d) Develop a connected network of public spaces, essential services, and multimodal transportation options that prioritize pedestrians and cyclists with seamless access to the GO Station.
- e) Advance sustainability and climate resilience through compact urban form, green infrastructure and spaces.
- f) Accommodate growth by achieving the provincially-mandated minimum density target of 150 people and jobs per hectare.

6.22.3 Major Transit Station Area Corridors Policies

The distribution of population and employment growth within the Major Transit Station Area (MTSA) is planned through four distinct corridor categories, as identified on Schedule B1-A of this Official Plan. Each corridor reflects unique characteristics related to context, land use, and built form, and is intended to accommodate varying levels of residential development, density, and growth in a manner that is tailored to its character and constraints, as generally illustrated in Figure 1:

MTSA Corridor	Units	UPH*	Residents	Commercial Area	Jobs
<i>Dissette Street</i>	3,990	300	7,600	5,900 sq. m.	121
<i>Holland Street</i>	860	200	1,500	6,700 sq. m.	107
<i>Residential Edges (north & south)</i>	360	40	1,000	0	0
<i>Bridge Street</i>	0	0	0	3,200 sq. m.	58
<i>Total Net Growth</i>	5,210	-	10,100	15,800 sq. m.	286

Figure 1 – Net People and Jobs estimates per corridor to 2051

*UPH- stands for Units per Net Hectare

The Dissette Street Corridor is expected to experience the highest level of growth, followed by the Holland Street Corridor. The residential edges of the MTSA are comprised of established neighbourhoods and are therefore not anticipated to accommodate significant additional growth over. Due to natural hazard constraints and flood risk, residential development is not permitted within the Bridge Street Corridor until the Town has implemented the measures outlined in policy 6.22.4.2 a) vii). In the interim, commercial development may proceed in select southwestern portions of the corridor, subject to the requirements set out in Section 6.22.5. Figure 2 illustrates the projected contribution of each corridor to the overall growth of the Major Transit Station Area (MTSA), which is anticipated to accommodate approximately 10,100 additional residents and 290 additional jobs by 2051:

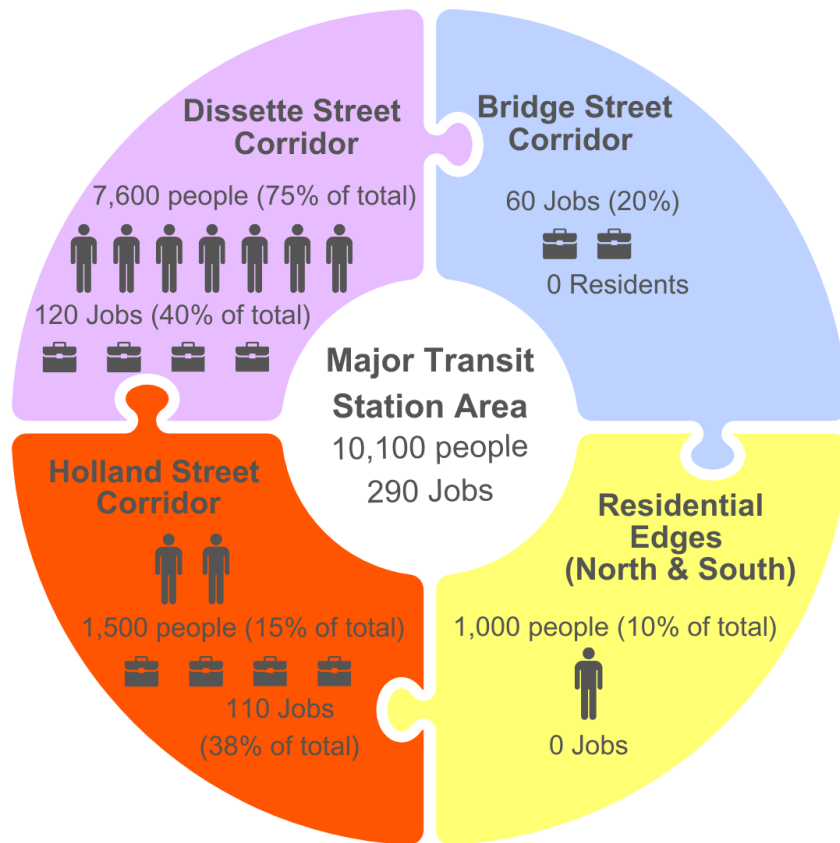


Figure 2 - The estimated proportion of people and jobs to be added per corridor by 2051

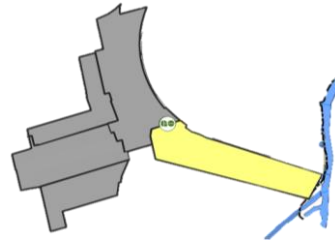
The policies in this Section are organized according to the corridor categories identified on Schedule B1-A, as follows:

- Policies applicable to the Bridge Street Corridor where commercial development is permitted;
- Policies for portions of the Bridge Street Corridor and other corridors located within the floodplain;
- Policies for the Holland Street Corridor;
- Policies for the Dissette Street Corridor; and
- General area-wide policies applicable to the entire Major Transit Station Area.

These policies shall be read in conjunction with all other applicable policies of this Plan. In the event of a conflict, the policies contained in this Section shall prevail.

6.22.4 Bridge Street Corridor Policies

The Bridge Street Corridor forms the eastern portion of the Town's Major Transit Station Area (MTSA), encompassing the Bradford GO Station, as shown on Schedule B1-A. Located just west of the Holland River, this corridor has long been a centre of mobility, commerce, and regional connectivity.



Its historical significance is rooted in its proximity to the Holland River—a vital extension of the Carrying Place Trail, a major Indigenous trade and travel route used by the Wendat (Huron), Haudenosaunee (including the Onondowahgah, or Seneca), Anishinaabe, and Mississaugas. The trail connected Lake Ontario to Lake Simcoe and beyond, with its most documented route passing through the west Holland River adjacent to what is now the Bridge Street Corridor. Travellers journeyed north from the shores of Lake Ontario via the Humber River valley, crossed the Holland Marsh, and launched canoes onto the Holland River en route to Lake Simcoe, Georgian Bay, or westward to Michilimackinac—an important Indigenous trading centre at the confluence of Lakes Huron and Michigan. Along this extensive travel network, goods, knowledge, and culture were exchanged, making it one of the busiest trading routes in Southern Ontario. Through this connection, the Bridge Street Corridor became part of an enduring system of movement and exchange, establishing its legacy as a place of cultural, economic, and regional significance.

With the arrival of European settlers in the 18th and 19th centuries, the area remained a transportation hub, bolstered by the development of roads and the Northern Railway. The opening of Bradford's first train station in 1853—on the site now occupied by the Bradford GO Station—strengthened the corridor's role as a conduit for agricultural goods from surrounding farms and the Holland Marsh to expanding urban markets.

This legacy of connectivity informs the vision for the corridor's future: to once again function as a vital connector—linking people to regional opportunities while fostering a distinct sense of place that honours the past and supports a vibrant, inclusive future. Anchored by the Bradford GO Station, the Bridge Street Corridor is expected to accommodate approximately 60 new jobs by 2051. Through compact, human-scaled development and thoughtful urban design, the corridor will support a dynamic, transit-oriented environment with a vibrant pedestrian realm—inviting people to walk, bike, work, and gather in spaces that are accessible, comfortable, and connected. As it evolves, the corridor will continue to reflect its historic role as a place of movement, interaction, and cultural exchange.

6.22.4.1 Bridge Street Corridor Commercial Areas

In recognition of the Bridge Street Corridor's legacy and longstanding cultural and historical role as a key route for facilitating commerce and connectivity to regional and urban markets, commercial uses are permitted in select areas, subject to the policies outlined below:

- a) Notwithstanding the natural hazard and floodplain policies of this Official Plan, the following policies apply to the southwestern portion of lands in the Bridge Street Corridor, as identified in Figure 3 below. This area is located approximately 320 metres west from the Holland River shoreline, adjacent to Bridge Street:
 - i) A full range of retail, service commercial, recreational, and small-scale office uses may be permitted, subject to a maximum development depth of 67 metres from the lot line fronting Bridge Street and shall contribute to the projected number of jobs for the Major Transit Station Area.
 - ii) All redevelopment shall be floodproofed to an elevation determined by, and to the satisfaction of, the Lake Simcoe Region Conservation Authority.
 - iii) A minimum 15-metre landscaped buffer shall be provided adjacent to the drainage ditch generally located along the southern boundary of the Major Transit Station Area. The final width and design of the buffer shall be subject to the satisfaction of the Lake Simcoe Region Conservation Authority.
 - iv) Only surface parking at grade shall be permitted in the area located between the 67-metre maximum development depth and the 15-metre landscaped buffer to the south. The design and placement of such parking shall be to the satisfaction of the Lake Simcoe Region Conservation Authority.
 - v) Residential uses shall be prohibited on these lands.

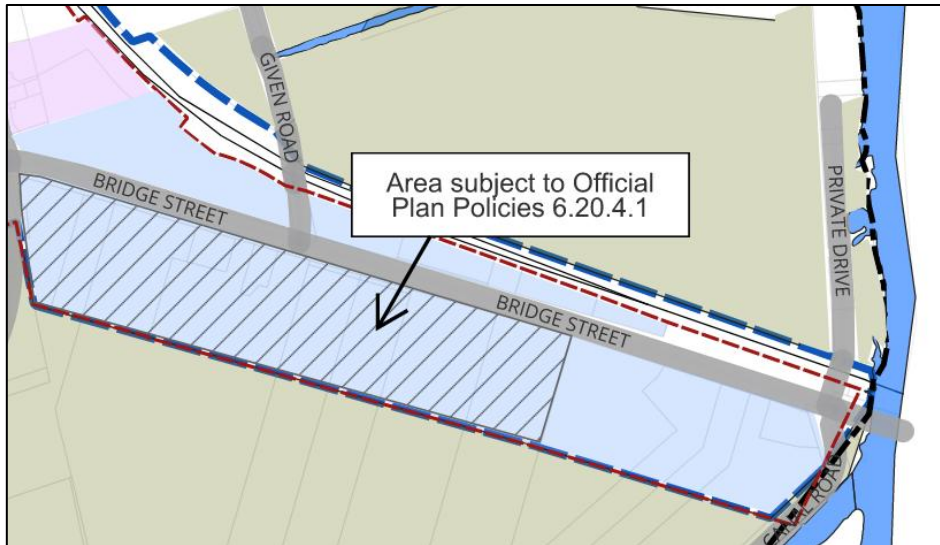


Figure 3 - Commercially Permitted Uses areas on Bridge Street

- b) For all other lands within the Bridge Street Corridor that are not subject to the permissions outlined in Section 6.22.4.1, the policies of Section 6.22.4.2 and 6.22.4.3 shall apply.

6.22.4.2 Floodplain Overlay

As shown on Schedule B1-A, the entirety of the Bridge Street Corridor—along with portions of the Dissette Street Corridor and the South Residential Edge Corridor—is located within a regulated floodplain and is subject to natural hazard policies. The flood susceptibility of these areas is intrinsically linked to the environmental history of the surrounding Holland Marsh, which has shaped the low-lying topography and ongoing flood risk.

Historically, the land surrounding Bridge Street was part of a vast wetland basin—an estuary of prehistoric Lake Algonquin near the end of the last Ice Age. As the lake receded toward its present-day boundaries at Lakes Simcoe and Huron, the area remained saturated, forming a rich organic wetland. This landscape is now known as the Holland Marsh—a unique ecological area historically used by Indigenous communities for fishing, hunting, and gathering natural resources such as cranberry bushes, tamarack, and cedar.

In the 19th century, European settlers transformed the Holland Marsh into one of Ontario’s most productive agricultural areas through an engineered drainage system of canals and pumping stations. The Bridge Street Corridor, directly adjacent to and shaped by this infrastructure, emerged as a key access point for the transportation of agricultural goods from local farms to growing urban markets. Today, while the canal system continues to support drainage for farming, it also accommodates recreational uses such

as paddling and informal trail connections along the Holland River and canal network near Bridge Street.

Despite engineered drainage systems, the Corridor remains flood-prone due to its low-lying topography and proximity to the Marsh. In accordance with provincial policy, development in these areas is generally discouraged. Until the Town implements the flood mitigation measures identified in Policy 6.22.4.2 a) vii), development within these flood-prone areas will remain limited.

Nonetheless, the Bridge Street Corridor's unique setting—at the intersection of historic trading routes, farmland, river access, and the regional canal system—offers valuable opportunities to support nature-based and water-oriented recreation. With thoughtful planning, these lands can contribute to a resilient and connected community that respects the ecological legacy of the Marsh while enhancing environmental stewardship and public access.

The policies in this section apply to lands within the floodplain as identified on Schedule B1-A and shall be read in conjunction with the Natural Hazard policies outlined in Section 3.10.5 of this Plan.

- a) The following uses are not permitted within lands located in a flood plain:
 - i) an institutional use including but not limited to places of worship, hospitals, long-term care homes, retirement homes, preschools, school nurseries, day cares, schools and related uses which would pose a significant threat to the safety of the inhabitants if involved in an emergency evacuation situation as a result of flooding or failure of floodproofing measures;
 - ii) an essential emergency service such as that provided by fire, police, and ambulance stations and electrical sub-stations which would be impaired during a flood emergency as a result of flooding or failure of floodproofing measures;
 - iii) uses associated with the disposal, manufacturing, treatment or storage of hazardous substances, including the treatment, collection and disposal of sewage, which would pose an unacceptable threat to public safety if they were to escape their normal containment/use as a result of flooding or failure of floodproofing measures;
 - iv) New underground parking;

- v) Overnight parking where safe ingress and egress as defined for roadways is not available;
 - vi) Until such time that the measures outlined in policy 6.22.4.2 a) vii) are undertaken by the town, residential uses are prohibited within all areas of the Bridge Street Corridor;
 - vii) The Town shall in consultation with the Ministries of Natural Resources and Municipal Affairs, the County of Simcoe, and the Conservation Authority, conduct the requisite studies and analysis to comprehensively evaluate the natural hazard constraints present on lands within the Bridge Street Corridor and the floodplain and assess the feasibility of establishing parameters for the potential future mixed-use redevelopment of the area.
- b) Development and site alteration shall not be permitted within:
- i) areas that would be rendered inaccessible to people and vehicles during times of flooding hazards, and/or erosion hazards, unless it has been demonstrated that the site has safe access appropriate for the nature of the development and the natural hazard; and
 - ii) a floodway regardless of whether the area of inundation contains high points of land not subject to flooding.
- c) Despite policy 6.22.4.2 b), development and site alteration may be permitted in certain areas associated with the flooding hazard where through consultation with the Lake Simcoe Region Conservation Authority and in accordance with the applicable requirements under the Provincial Planning Statement, Lake Simcoe Protection Plan, South Georgian Bay Lake Simcoe Source Protection Plan and the Conservation Authorities Act, if it is determined:
- i) That the development is limited to uses which by their nature must locate within the floodway, including flood and/or erosion control works or minor additions or passive non-structural uses which do not affect flood flows to the satisfaction of the Conservation Authority;
 - ii) The Conservation Authority, along with the Town and relevant agencies, must be satisfied that the proposed development will not create conditions that pose a risk to public health or safety, damage property in the event of a

natural hazard, or impede safe access for people, vehicles, and emergency services during a flood; and,

- iii) The Conservation Authority is of the opinion that the activity is not likely to affect the control of flooding, erosion or unstable soil or bedrock and meets the prescribed requirements and regulations

6.22.4.3 Bridge Street Corridor Lands in Floodplain

Despite the natural hazard policy limitations applicable, Bridge Street Corridor's unique location—at the interface of historic agricultural land, river access, and the regional canal system—presents opportunities to support nature-based and water-oriented recreational activities for the community. With careful planning, these lands can contribute to a resilient, connected community that respects the ecological legacy of the Marsh while enhancing public access and environmental stewardship.

Until the Town undertakes the measures outlined in Policy 6.22.4.2 a) vii., steps shall be taken to create a distinct sense of place and identity for the Bridge Street Corridor as the eastern gateway to the Town. Notwithstanding the policies in section 6.22.4.1 on Commercial use area permissions on the Bridge Street Corridor, the following policies apply to all lands within the Bridge Street Corridor:

- a) The Town shall promote enhanced Connectivity and active transportation opportunities through the Bridge Street Corridor by:
 - i) Developing pedestrian and cycling routes along Bridge Street that integrate with the broader active transportation network;
 - ii) Establishing a comprehensive active transportation network that provides direct, safe connections between the GO Station, other public transit stops, corridors, multi-use trails, public spaces, and other key destinations;
 - iii) Promoting safe, accessible pedestrian walkways with sufficient width to accommodate mobility aids.
- b) The Town shall ensure driveways along Bridge Street are minimized to maintain continuous, uninterrupted pedestrian and cycling pathways;
- c) The Town shall encourage the consolidation of vehicle access points where feasible to enhance building frontages,

improve front yard continuity, and reduce disruptions to pedestrian routes;

- d) The Town shall identify and pursue opportunities to enhance or establish new recreational and tourism-focused trail connections within and around the Bridge Street Corridor by:
 - i) Facilitating trail and water-based connections between the Bridge Street Corridor and the historical route of the Carrying Place Trail. This may include the establishment of multi-use pathways through the Holland Marsh that utilize the existing canal dykes where feasible, and public road rights-of-way where canal access is restricted or conflicts with privately owned agricultural lands.
 - ii) Supporting enhanced water-based recreational activities such as canoeing and paddling along the Holland River and its associated canal and dyke systems, with access provided from the Bridge Street Corridor. In alignment with the Town's Trails Master Plan, this may include exploring the development of multi-use trails along reconstructed dykes on both the North and South Canal branches of the Holland River, such as:
 - a. A North Canal route, extending west from Bridge Street and Canal Road, running parallel to Canal Road and the North Canal, and connecting south to the South Canal at West Canal Bank Road (approximately 24 km in total length).
 - b. South Canal route, beginning at Canal Road and West Bank Canal Road, continuing east along South Bank Canal Road and Woodchoppers Lane, then departing the road system to follow the canal before rejoining at Pumphouse Road, ultimately merging with the North Canal trail at Canal Street to create a continuous loop.
 - iii) Recognizing and supporting the cultural and recreational importance of the Marsh Mash Canoe and Kayak Race, one of Ontario's oldest marathon canoe races, which utilizes the canal system surrounding the Holland Marsh.
 - iv) The Town shall collaborate with event organizers, local stakeholders, and Indigenous communities to celebrate the historical significance of the Holland Marsh and its role in regional connectivity and commerce and

community partners to promote the cultural and recreational significance of the canal system and maintain access and infrastructure that supports ongoing public, recreational and traditional uses of the waterways.

- e) Providing or enhancing cycling routes that follow the canal system and connect to the Bridge Street Corridor.
- f) Incorporating support amenities into the trail system, such as trailheads with modest floating docks, rest areas, and interpretive features.
- g) Establishing a primary trailhead entrance along Bridge Street, including self-guided interpretive signage; and
- h) In consultation with the Lake Simcoe Region Conservation Authority, exploring the potential for a “rail-with-trail” cycling route from the Bradford GO Station (Bridge Street and Given Road), extending northward along the railway corridor with the potential to connect to the Scanlon Creek Conservation Area, subject to appropriate design and safety standards.
- i) The Town shall also support and facilitate the establishment and integration of public and privately owned art installations—such as standalone pieces, architectural features, and landscape elements—that enhance the public realm and reinforce the unique identity of the Bridge Street Corridor as the eastern gateway to the Town. Public art should be strategically located and reflect the area’s cultural legacy, including:
 - i) Its role in the historic Carrying Place Trail;
 - ii) Its function as a marketplace for Marsh-grown produce;
 - iii) Its historic significance as a hub of regional connectivity, commerce, and economic activity; and,
 - iv) As part of this effort, the Town shall promote Indigenous placemaking through the incorporation of culturally significant design features and public art co-developed with Indigenous communities. These elements shall celebrate Indigenous cultures, stories, values, and knowledge systems, contributing to a more inclusive and meaningful sense of place.

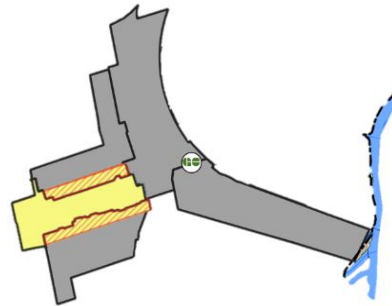
6.22.4.4 Bradford GO Station

The following policies shall apply to the Bradford GO Station located within both the Bridge Street Corridor, with a smaller portion on Dissette Street Corridor.

- a) Parking and passenger pick-up/drop-off areas serving the GO Station shall prioritize universal accessibility and support seamless transitions between automobile use and active transportation;
- b) A comprehensive active transportation network that provides direct, safe connections between the GO Station, transit stops, corridors, multi-use trails, public spaces, community hubs and other key destinations shall be established;
- c) Safe, accessible, and well lit pedestrian walkways with sufficient width to accommodate mobility aids shall be provided to the GO station from key nodes;
- d) Secure and ample bicycle parking shall be provided at GO Station entrances, with sheltered bike storage integrated into the station design and positioned in highly visible locations near platform access points.
- e) Passenger amenities should be integrated into buildings and open spaces adjacent to the transit stop.

6.22.5 Holland Street Corridor

The Holland Street Corridor, consists of approximately 4 hectares of net land area, and forms the central portion of the Town's *Major Transit Station Area (MTSA)*, as shown on Schedule B1-A. As one of the first streets established in the former Village of Bradford in the mid-19th century, Holland Street has served as the Town's main street and destination, offering goods, services, and amenities to both local residents and visitors from the surrounding rural communities.



This corridor is home to several historic and heritage buildings that reflect the Town's early settler development and cultural evolution. The conservation, adaptive reuse, and integration of these heritage resources play a vital role in enriching the daily experience of residents and visitors, and contribute to the Town's identity and sense of place. Preserving cultural and natural heritage is a key element of sustainable development

and placemaking. As the Holland Street Corridor continues to evolve, intensification must be carefully balanced with the protection of its heritage properties, viewsheds, and historical identity as a community destination.

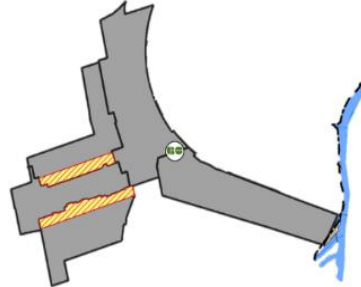
Identified as the second-highest intensification area within the MTSA, the Holland Street Corridor is planned to accommodate growth to the year 2051 with approximately 860 new dwellings, a minimum density of 200 units per net hectare, 1,500 residents, 5,900 square metres of new commercial floor area, and 110 jobs. Future redevelopment shall be guided by a commitment to maintaining the corridor's historical and cultural character, while supporting a vibrant, mixed-use environment that provides diverse housing, jobs, recreational spaces, and community amenities.

- a) Permitted uses within the Holland Street Corridor include:
 - i) High density housing units consisting of mixed use, mid-rise apartments with commercial uses including retail, restaurants, and services, provided at grade;
 - ii) Small-scale commercial, Cultural, art and entertainment uses, community and institutional uses are encouraged at grade in apartment buildings and properties
 - iii) Public uses including public service facilities, infrastructure, parks, recreation/community centres, urban squares, trails with signs to GO Station
- b) Residential developments within the Holland Street Corridor must achieve a minimum density of 200 units per net hectare;
- c) Minimum and maximum heights in Holland Street Corridor shall be in accordance with the height limitations shown on Schedule B-1A of this Plan;
- d) Opportunities for requesting an increase in the maximum permitted height may be pursued subject to the policies outlined in section 4.2.2 d) of this plan;
- e) The Town shall encourage the consolidation of contiguous lots to facilitate comprehensive and coordinated development. Lot assembly is supported where it results in more efficient use of land, improved site design, and enhanced compatibility with surrounding land uses and infrastructure.
- f) Existing buildings on properties that are either listed or designated under the Ontario Heritage Act, or adjacent to such

properties, contained on development sites must be assessed for their heritage value in accordance with policy 3.6.1 of this plan.

6.22.5.1 Holland Street Transition Overlay

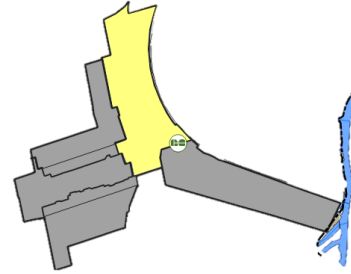
Given the proximity of properties within the Holland Street Corridor Transition Overlay to established neighbourhoods within the residential edges corridors, as shown on Schedule B1-A, the following policies apply to ensure compatibility and seamless integration is maintained between the two corridors:



- i) Residential development within the Transition Overlay shall contribute to achieving the overall minimum density target of 200 units per net hectare for the Holland Street Corridor.
- ii) Development shall incorporate a gradual transition in height, scale, and density from Holland Street toward adjacent low-density residential areas through step-backs, setbacks, and context-sensitive design.
- iii) Higher building heights shall be directed to sites fronting onto Holland Street, with lower-scale, ground-oriented forms permitted within the Transition Overlay to support compatibility with surrounding neighbourhoods.
- iv) A range of built forms and densities shall be provided to ensure a smooth transition between the Corridor and adjacent residential areas.
- v) Site and building design shall mitigate impacts on neighbouring properties by maintaining sunlight access, protecting privacy, and limiting overlook, shadowing, and light spill through appropriate massing, orientation, and screening.
- vi) Developments shall incorporate soft landscaping, fencing, and other buffering treatments to enhance livability and ensure appropriate residential interfaces.

6.22.6 Dissette Street Corridor

The Dissette Street Corridor is located north of the Bradford GO Station and consists of the northern portion of the Major Transit Station Area. The corridor comprises approximately 15 hectares of net land area, including a portion of the passenger pick-up/drop-off area and associated parking lot of the Bradford GO Station, as shown on Schedule B1-A.



Planned as the highest-density and most intensively developed corridor within the Major Transit Station Area, the Dissette Street Corridor shall support a vibrant, pedestrian-oriented and mixed-use community. The Corridor will accommodate the tallest buildings with a diverse mix of residential, office, institutional, and commercial uses. To enhance the public realm and pedestrian experience, developments shall incorporate ground-floor retail, Privately Owned Publicly Accessible Spaces (POPs), parks, trail connections, and wide sidewalks with integrated amenities.

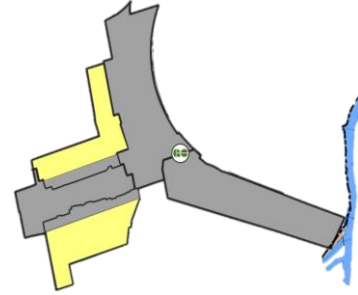
Development in this corridor will have consideration for the presence of existing industrial and major facilities, with the introduction of sensitive land uses to be phased and appropriately mitigated to ensure land use compatibility. Overall, this Corridor is planned to accommodate a total of approximately 3,990 dwellings, a density of 300 units per net hectare, 6,700 square metres of net new commercial floor area, and is expected to support a population of approximately 7,600 people and 121 jobs by 2051. The following policies shall apply to lands within the Dissette Street Corridor:

- a) Permitted Uses within the Dissette Corridor include:
 - i) Legally existing uses and buildings established prior to the adoption of this Plan, however, existing uses are intended to be redeveloped in conformity with this Plan over time;
 - ii) High-density mixed-use developments shall be permitted, consisting of above-grade residential apartments with ground-floor uses fronting Dissette Street that may include retail, restaurants, community services, offices, institutional uses, or public service facilities, such as medical offices or clinics.
 - iii) Residential developments within the Holland Street Corridor must achieve a minimum density of 300 units per net hectare;

- iv) Minimum and maximum heights in the Dissette Street Corridor shall be in accordance with the height limitations shown on Schedule B-1A of this Plan;
 - v) Opportunities for requesting an increase in the maximum permitted height may be pursued subject to the policies outlined in section 4.2.2 d) of this plan;
 - vi) Drive-throughs and new automotive service uses are prohibited;
- b) The tallest buildings shall be concentrated in closer proximity to the Bradford GO Station, where access to community amenities, facilities, transit, and active transportation routes is greatest and transition down toward residential edges;
 - c) Buildings in proximity to the Bradford GO Station are encouraged to enhance the public realm through high-quality design elements, including façade articulation, fenestration detailing, landmark architectural features, and the use of durable, high-quality materials. Developments shall also provide clear and direct pedestrian connections to the GO Station to support walkability and transit accessibility.
 - d) All development within 300 meters of heavy rail shall comply with the requirements of the Metrolinx/GO Transit Adjacent Development Guidelines;
 - e) The following policies apply to the development of lands adjacent to the railway right-of-way:
 - i) Development adjacent to the railway corridor may require compatibility assessments to address safety, noise, vibration, and other potential impacts; and,
 - ii) Warning clauses and environmental easements may be required through the planning process to notify future occupants of railway operations and to address operational emissions, particularly for developments within 300 metres of the railway corridor.

6.22.7 Residential Edges (North and South)

The Residential Edges Corridor consists of two separate areas in the northern and southern section of the Major Transit Station Area. This area is characterized by a mixture of established residential neighbourhoods, institutional uses and residential intensification projects.



The southern Residential Edge is also intended to accommodate the future Town Civic Centre. The sites within this area are intended to offer opportunities for a modest increase in density, accommodating a forecasted 1,000 additional residents by 2051. Permitted uses within the Residential Edges Corridor include:

- a) Legally existing uses and buildings established prior to the adoption of this Plan.
- b) A diverse mix of low to medium-density residential housing options including semi-detached dwellings, duplexes, triplexes townhouses, multiple unit dwellings and up to 10-unit walk-up apartments.
- c) Residential developments within the Residential Edges Corridors must achieve a minimum density of 40 units per net hectare.
- d) Parks, urban squares, Privately Owned Public Spaces (POPS), institutional uses including public service facilities, and neighbourhood commercial uses—such as convenience stores, personal service establishments, small-scale eating establishments, and other similar businesses that support the day-to-day needs of the local community—shall be permitted.
- e) Developments shall prioritize strong pedestrian connections, trails and walkways to the Bradford GO Station, adjacent corridors and encourage the integration of parks and open spaces with linkages to the active transportation network.
- f) Institutional uses including public services facilities, Neighbourhood commercial uses such as convenience stores, personal service establishments, small-scale eating establishments, and other similar convenience commercial uses that serve the day-to-day needs of the area.

6.22.8 Major Transit Station Area General Policies

a) Population and Employment

The overall Major Transit Station Area has a gross area of 62 hectares and is planned to with consideration of its existing population it is anticipated to accommodate a total population of 11,500 residents, 15,800 sq. m. of commercial floor area and an estimated total of 575 jobs.

<i>Growth to 2051 Category</i>	<i>2051</i>
<i>Total Population</i>	11,500
<i>Total Jobs</i>	1,300
<i>Total Gross Land Area (Hectare)</i>	62
<i>Residents and Jobs per Hectare</i>	200 people and jobs

b) Transportation

i) Transportation studies/future road networks:

- a. Collector and arterial roads will be designed to balance automobile, pedestrian and cycling activity, to contribute to the vision of a safe, transit-supportive, complete community with a pedestrian-oriented environment.
- b. In order to increase connectivity in the area, the Town will explore the feasibility of new road connections including:
 1. An extension of Scanlon Avenue east to Disette Street
 2. A new connection between Marshview Boulevard and Canal Road
- c. Privately owned streets and driveways will be designed to be consistent with the overall vision for the transportation network in the MTSA as well as Town engineering standards.
- d. Direct connections from private roadways to collector and arterial roads will only be permitted for shared access to a condominium block or to parking access for larger sites. Multiple individual connections from private roads or lanes shall be predominantly accessed from local roads.

ii) Active transportation network

- a. Development in the Bradford GO MTSA will maximize the connectivity of the street network for pedestrians and cyclists. This will be achieved by:
 1. Ensuring that gaps in the street network are minimized by the provision of strategically located sidewalk and pathway connections
 2. Seeking opportunities to enhance grid connectivity between new developments and the surrounding pedestrian and bicycle infrastructure
 3. Providing high-quality bicycle parking in safe, conveniently located areas.

iii) Transit facilities

- a. The Town will leverage existing transit connections to improve transit access between the GO Station and other parts of Bradford West Gwillimbury.

iv) Parking

- a. Notwithstanding any other policies in this Plan, development applications on lands located with the Bradford GO Major Transit Station Area will not be subject to minimum parking requirements.
- b. The Town encourages the redevelopment of surface parking lots along Holland and Dissette streets in order to achieve the minimum density target.

c) Housing

The Province of Ontario has authorized the use of Inclusionary Zoning within Major Transit Station Areas to require the provision of affordable housing units within new developments. To support the implementation of Inclusionary Zoning within the Bradford Major Transit Station Area:

- a. The Town shall conduct an Assessment Report, in accordance with provincial requirements, that analyzes demographics, income levels, housing supply, housing need and demand, average market prices and rents, and the potential impacts on the housing market. The findings of this report shall inform the consideration and potential implementation of Inclusionary Zoning within the Major

Transit Station Area through future secondary planning, equivalent processes, and subsequent zoning by-law amendments.

d) Public Realm

- a. Development shall provide direct, safe, and legible pedestrian and cycling connections to the Bradford GO Station and adjacent transit facilities, incorporating wayfinding features, weather protection, and defined gateways to enhance the user experience.
- b. All development shall be compact in form, pedestrian-oriented, and designed to frame the public realm with active frontages, frequent pedestrian entrances, and strong connections to surrounding open spaces and transit infrastructure.
- c. Streets, boulevards, and rights-of-way shall be designed to support multi-modal mobility and high-quality public spaces, including continuous sidewalks or multi-use paths on both sides of the street, connected cycling infrastructure, pedestrian-scaled lighting using full cut-off fixtures, and street trees or landscaping for shade, comfort, and visual interest.
- d. Development shall incorporate public or semi-public gathering spaces—such as plazas, urban squares, or Privately Owned Public Spaces (POPS)—that promote community interaction, provide social and recreational functions, and activate the corridor year-round.
- e. A fine-grained, permeable street network with shorter blocks and frequent, controlled pedestrian crossings shall be encouraged to promote walkability and support safe, flexible travel options.
- f. Roadway design shall integrate traffic calming measures and prioritize pedestrian and cyclist safety while maintaining visually attractive and functional streetscapes.
- g. The public realm shall connect to an integrated system of parks, trails, and open spaces, providing year-round opportunities for walking, cycling, recreation, and ecological connections to the Holland Marsh, canal system, and surrounding neighbourhoods.

- h. Development shall contribute to cohesive, high-quality streetscapes through the use of durable materials, coordinated street furnishings, and inclusive design features that support universal accessibility and equitable use of public

6.22.9 Implementation and Monitoring

- a) The boundaries of the Bradford GO Major Transit Station Area as shown on Schedules B1 and B1-B, are subject to minor refinements which will not require an amendment to this Plan, provided that the achievement of the overall Vision for the area is not negatively affected.
- b) The minimum density requirement is measured within all of the lands delineated in the MTSA, inclusive of any environmental features, constraints, and rights-of-way for transportation or other linear infrastructure.
- c) The minimum density requirement will be achieved based on all existing uses and new development over the long-term. Individual developments within the MTSA are not required to meet the minimum target.
- d) All development applications shall demonstrate how proposals support the overall gross density target for the MTSA as well as the Town's long-term Vision for the area.
- e) As part of future planning or implementation projects, the Town may assess establishing a Community Planning Permit System (CPPS) within the MTSA to facilitate streamlined, transit-supportive development and coordinated design outcomes, in accordance with Section 70.2 of the Planning Act and its regulations.

PART C – THE APPENDIX

Record of Bradford West Gwillimbury Council Action

To be completed.